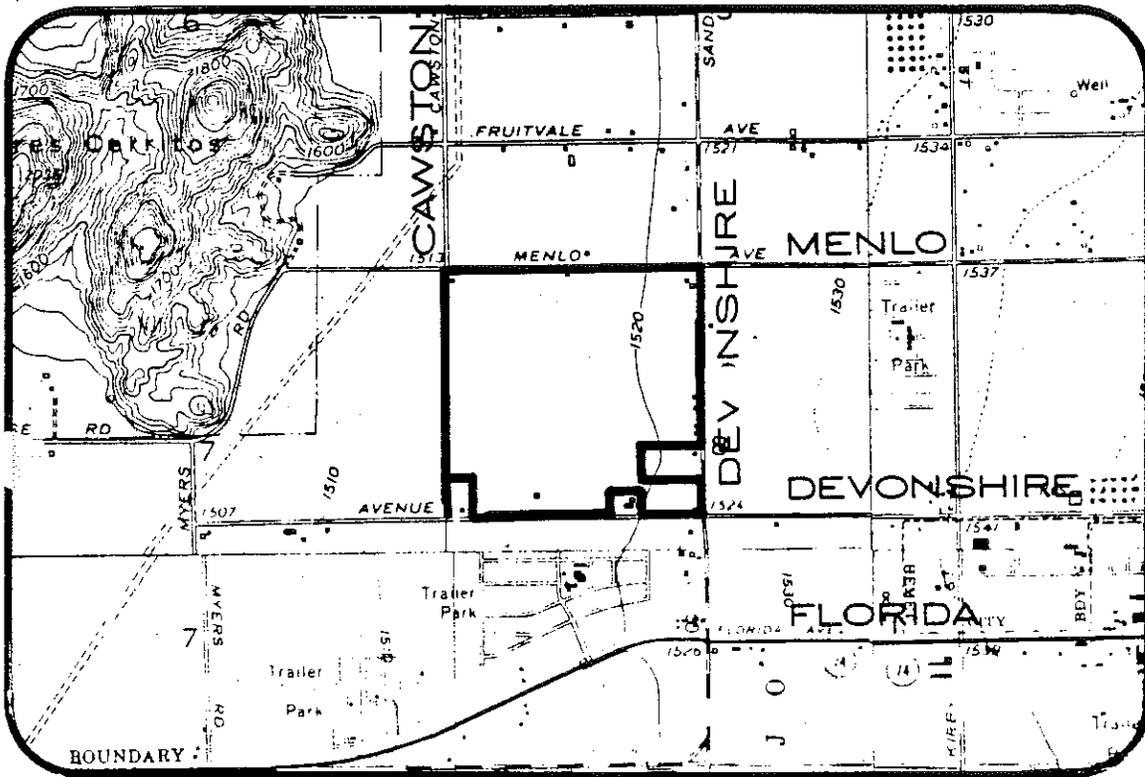


ARTHOFER
SP 85-01

*Vicinity Map
Specific Plan
Resolutions*

VICINITY MAP

300' RADIUS MAP
CASE N° S.P. 85-01



DESCRIPTION

PORTION OF THE NORTHWEST QUARTER
OF SEC. 8, T.5S., R.1W., S.B.B.&M.

APPLICANT

NORTHRIDGE INVESTMENTS LTD.
3092 CHESTNUT AVENUE
RIVERSIDE CA 92502 (714) 924-1061

APN

441-160-008-014, EXCL. 011
441-170-001-011 EXCL. 003, 012

AREA

147.99 ACRES NET

LEGEND

ORDINANCE NO. 1289

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF HEMET, CALIFORNIA, APPROVING ZONE CHANGE 88-15 AMENDING SPECIFIC PLAN NO. 85-01, FROM (TR-20 STANDARDS) TO (R-3 MULTIPLE FAMILY RESIDENTIAL STANDARDS), FOR PROPERTY LOCATED ON THE WEST SIDE OF SANDERSON AVENUE, 858 ± FEET NORTH OF DEVONSHIRE AVENUE

The City Council of the City of Hemet, California, does hereby ordain as follows:

SECTION ONE:

The Official Zoning Map, Atlas Sheet No. 113G, as amended, is hereby amended by amending Specific Plan No. 85-01 from TR-20 Standards, to R-3 Multiple Family Residential Standards, with a requirement for a single development plan for the six areas (9.33 ± acres), for property generally located:

West side of north Sanderson Avenue, 858 ± feet north of Devonshire Avenue

SECTION TWO:

The City Council makes the following findings:

SPECIFIC PLAN

The Specific Plan effectively limits the density which can be achieved; The General Plan has rather broad ranges of density which would be limited to specific densities as approved in the Specific Plan. Conventional zoning development standards are proposed for each land use category:

AREA 1

Area 1 is proposed for single family residential subdivisions and shall be developed in accordance with the R-1 Single Family Residential (Chapter 12, Title 17, Zoning) Development Standards with R-1-5, R-1-6, and R-1-8 densities as delineated on Tentative Tract Map No. 22189. (Ordinance No. 1252) (ZC 87-14)

AREA 2 AND 3

Area 2 and 3 shall be developed for apartments and shall be developed in accordance with the R-3 Multiple Family Residential (Chapter 15, Title 17, Zoning) Development Standards with the density not to exceed 25 units per net acre after dedication for required street rights-of-way. (Ordinance No. 1130) (SP85-01)

AREA 4

Area 4 shall be developed for senior citizen apartments and shall be developed in accordance with the SR-3 Senior Apartment Residential (Chapter 18, Title 17, Zoning) Development Standards with the density not to exceed 40 units per net acre after dedication for required street rights-of-way. (Ordinance No. 1130) (SP85-01)

AREA 5

Area 5 shall be developed for a single family residential subdivision in accordance with the R-1 Standards, as modified. (Ordinance No. 1256) (ZC87-19)

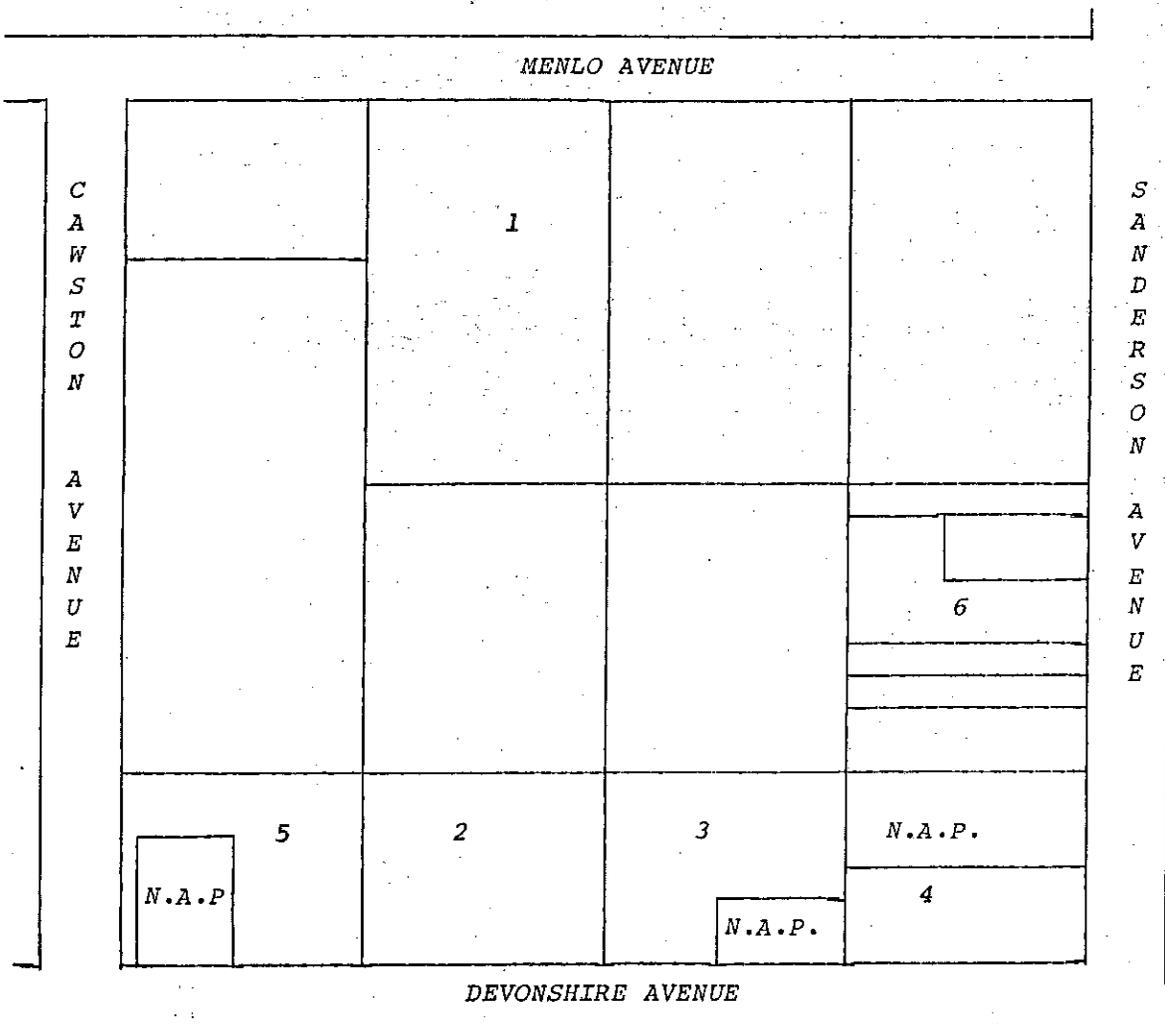
AREA 6

Area 6 shall be developed for apartments and shall be developed in accordance with the R-3 Multiple Family Residential (Chapter 15, Title 17, Zoning) Development Standards with the density not to exceed 17 units per net acre after dedication for required street rights-of-way. There shall be a single development plan approved for all six parcels in this area prior to any construction. (ZC 88-15)

SECTION THREE:

A map of the Specific Plan areas of the property described in Section One herein is as follows:

SPECIFIC PLAN AREAS:
NOT TO SCALE



SECTION FOUR:

Conditions of Approval are herein attached as Exhibit "A".

Introduced at the regular meeting of May 24, 1988.

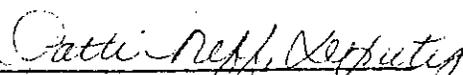
Enacted at the regular meeting of June 14, 1988
by the following vote:

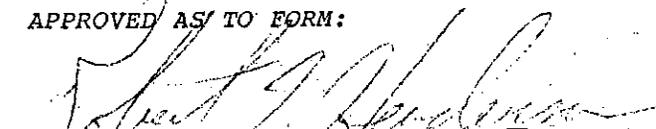
- AYES: Council Members Garrett, Herron, Quinn, Nishino
- NOES: None
- ABSTAIN: None
- ABSENT: Baskett


 Ken Nishino
 Mayor, City of Hemet

ATTEST:

APPROVED AS TO FORM:


 Datti Neff, Deputy
 City Clerk, City of Hemet


 Robert T. Henderson, City Attorney

1 ORDINANCE NO. 1289

2 EXHIBIT "A"

3 SPECIFIC PLAN NO. 85-01
4 CONDITIONS OF APPROVAL
AS AMENDED BY ZONE CHANGE NO. 88-15

- 5 1. Detailed site plans and/or tract maps for each phase shall be
6 submitted to the Planning Commission for review and approval.
- 7 2. Full signalization at the corner of Sanderson and Devonshire
8 shall be installed at the start of Phase I. If feasible, a
reimbursement agreement shall be worked out to the satisfaction
9 of the City Engineer.
- 10 3. The Master-Planned Flood Control Channel proposed north of Menlo
11 Avenue, west of Sanderson Avenue, shall be relocated south of
12 Menlo Avenue, if feasible, as determined by the City Engineer
after the appropriate hydraulic and hydrologic studies have been
13 completed. The entire channel shall be a green belt moving
14 through Area 1 rather than a concrete-lined channel.
- 15 4. Vehicular access to Sanderson is prohibited except via Devonshire
16 Avenue or Menlo Avenue, except a single street being allowed for
17 Area 6.

18 -END-

19 CITY OF HEMET
20 MAY 24, 1988
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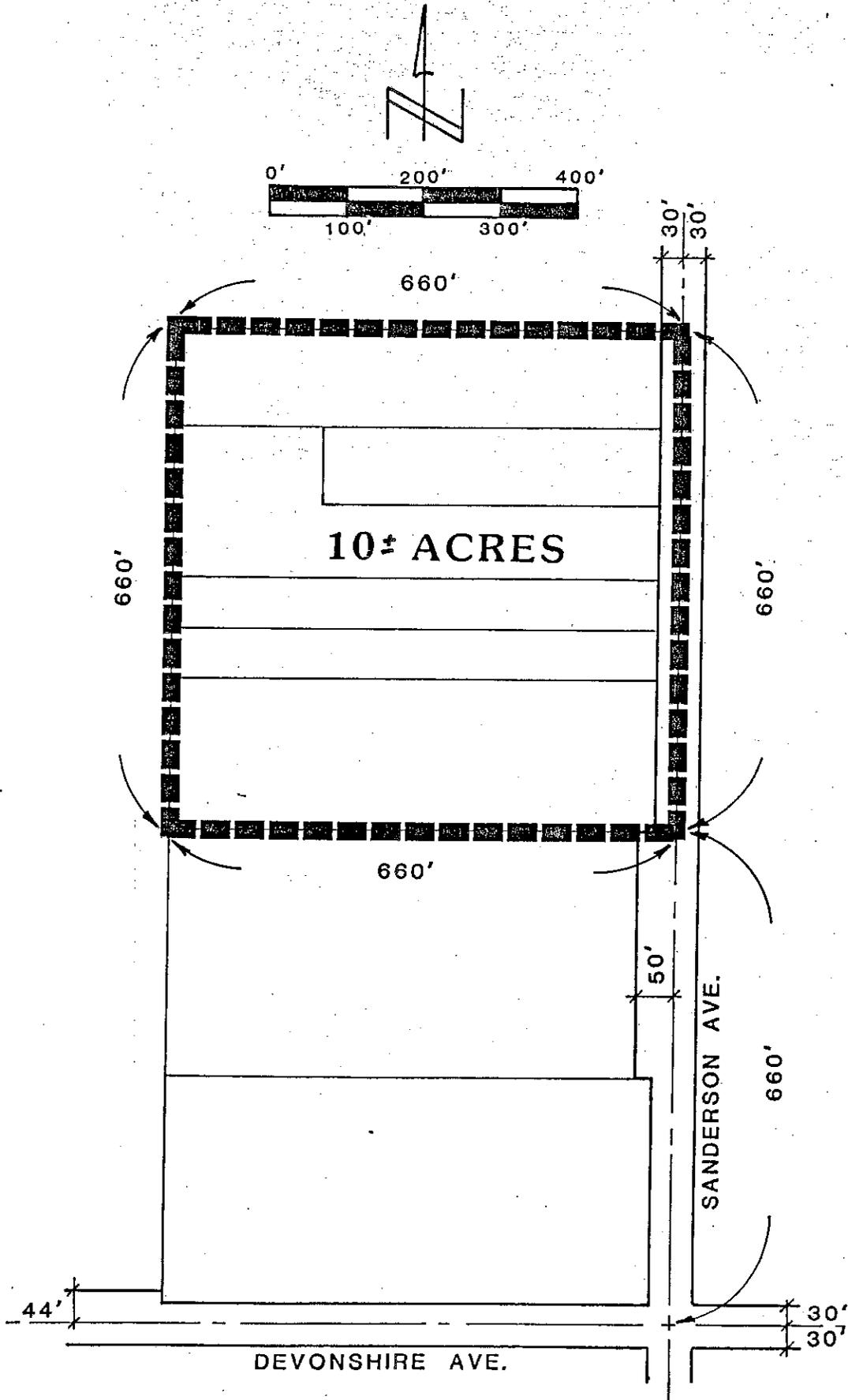
ZONE CHANGE NO. 88-15

FROM: SP 8501 (TR-20)

TO: SP 8501 (R-3)

ORD.

SUBJECT PROPERTY 



AS#113

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ORDINANCE NO. 1280

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF HEMET, CALIFORNIA, APPROVING ZONE CHANGE 88-07 AN AMENDMENT TO SPECIFIC PLAN NO. 85-01 DEVELOPMENT STANDARDS ALLOWING DEVELOPMENT OF SMALL LOT SINGLE FAMILY HOMES UTILIZING SIDEYARD EASEMENTS, RATHER THAN ZERO SIDE YARD STANDARDS

The City Council of the City of Hemet, California, does hereby ordain as follows:

SECTION ONE:

The Official Zoning Map AS #112, is hereby amended by changing the zone of the subject property by amending Specific Plan No. 85-01 Development Standards allowing development of small lot single family homes utilizing sideyard easements, rather than zero side yard standards, for the following described property:

7.5 + acres located at the northeast corner of Devonshire Avenue and Cawston Avenue

More particularly described as:

LEGAL:

PARCEL 1 OF PARCEL MAP NO. 21217 IN THE CITY OF HEMET, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN ON A MAP RECORDED IN BOOK 140, OF PARCEL MAPS, PAGES 63 THROUGH 64, INCLUSIVE, RECORDS OF SAID COUNTY.

SECTION TWO:

A map of the property described in Section One herein is attached hereto as Exhibit "A".

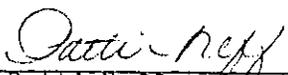
Introduced at the regular meeting of April 26, 1988.

Enacted at the regular meeting of May 10, 1988 by the following vote:

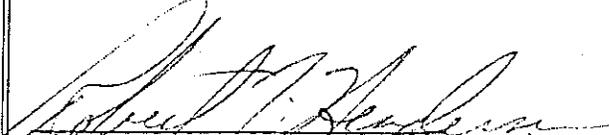
AYES: Council Members Garrett, Herron, Quinn, Nishino
NOES: None
ABSTAIN: None
ABSENT: Baskett


Ken Nishino
Mayor, City of Hemet

ATTEST:


Patti Neff, Deputy
City Clerk, City of Hemet

APPROVED AS TO FORM:


Robert T. Henderson, City Attorney

1 ORDINANCE NO.

2 SPECIFIC PLAN NO. 85-01
3 TRACT MAP NO. 22252
4 AMENDED DEVELOPMENT STANDARDS (ZC88-07)

5 Sections:

- 6 I. PURPOSE
7 II. USES
8 III. AREA
9 IV. YARD REQUIREMENTS
10 V. PLACEMENT OF BUILDINGS
11 VI. LOT WIDTH AND DEPTH
12 VII. HEIGHT
13 VIII. BUILDING AREA
14 IX. PERMISSIBLE LOT COVERAGE
15 X. OFF-STREET PARKING AND LOADING
16 XI. STORAGE AREA
17 XII. WALLS

- 18 I. PURPOSE - The purpose of these standards is to provide for the
19 development of small lot single family homes utilizing sideyard
20 easements.
- 21 II. USES PERMITTED - Only the following uses are permitted as
22 hereinafter specifically provided and allowed:
- 23 A. One-family dwellings;
- 24 B. Accessory buildings and structures subject to the provisions of
25 SubSection IV. herein and Section 20013, Chapter 1, Title 17,
26 of the Hemet Municipal Code.
- 27 C. Home occupations as prescribed and regulated in Section 20401,
28 Chapter 5, Title 17 of the Hemet Municipal Code.
- 29 D. Temporary uses as prescribed and regulated in Section 20402,
30 Chapter 5, Title 17, of the Hemet Municipal Code.
- 31 III. AREA - The minimum required lot area shall be 4,140 square feet.
- 32 IV. YARD REQUIREMENTS
- A. Front
1. 20-foot minimum; Garage doors shall be segmented overhead
rolltype with automatic openers.
- B. Side
1. Side yard and easement as shown in Figure 1.

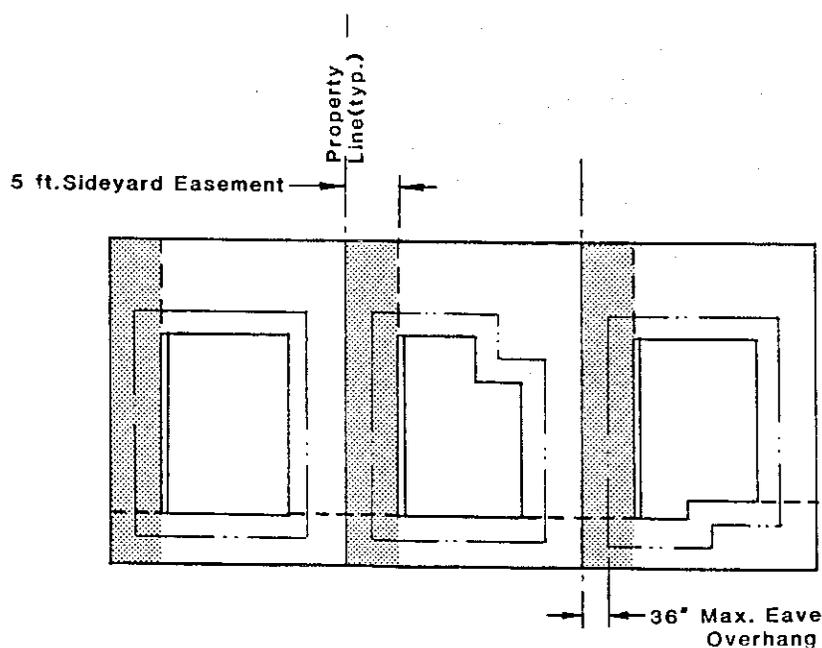
ORDINANCE NO.

SPECIFIC PLAN NO. 85-01, TRACT MAP NO. 22252

Amended Development Standards (ZC88-07)

Page 2

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Minimum five-foot (5') wide non-buildable (with exception of eave and roof gutter overhang) use easement granted to adjacent property owner. Opposite side yard shall be sevenfoot (7') wide minimum. Said easement shall be for maintenance, drainage, and eave encroachment purposes, as set forth in the development's CC & R's.

No window or door openings within five feet (5'), fire resistive construction not required.

Fence line

2. Side street setback - ten-feet (10') minimum

C. Rear

Every lot shall have a rear yard of not less than fifteen-feet (15') in depth.

V. Placement of Accessory Buildings - Placement of accessory buildings shall be as prescribed in Section 20013, Chapter 1, Title 17, of the Hemet Municipal Code.

VI. Lot Width and Depth - Every lot shall have a minimum width of forty-six feet (46') and a minimum depth of eight-three feet (83').

VII. HEIGHT - No building shall exceed one story in height or fifteen feet (15'), whichever is the lesser.

VIII. BUILDING AREA - The main residential building shall have a floor area of not less than eleven hundred square feet, excluding garage.

IX. PERMISSIBLE LOT COVERAGE - All buildings, including accessory buildings and structures, shall not cover more than forty-five percent (45%) of the area of the lot. Of the remaining area, ten percent (10%) shall be live landscaping.

X. OFF STREET PARKING AND LOADING

A. One 2-car garage per dwelling unit.

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B. No person shall stop, park or leave standing any aircraft, boat, bus, cab-over camper, motorhome or travel trailer, or any other vehicle or part thereof incapable of movement under its own power, whether attended or unattended, in either of the following places:

1. Within twenty-feet (20') of the curb or street edge of any street.
2. In any required yard.

XI. STORAGE AREA - An indoor, enclosed storage area of not less than thirty-two (32) square feet of floor area shall be provided. The storage area shall have a minimum depth of three feet (3') and a minimum height of eight-feet (8').

XII. WALLS - All rear and sideyards shall have a masonry wall along the property lines.

-END-
CITY OF HEMET
APRIL 26, 1988

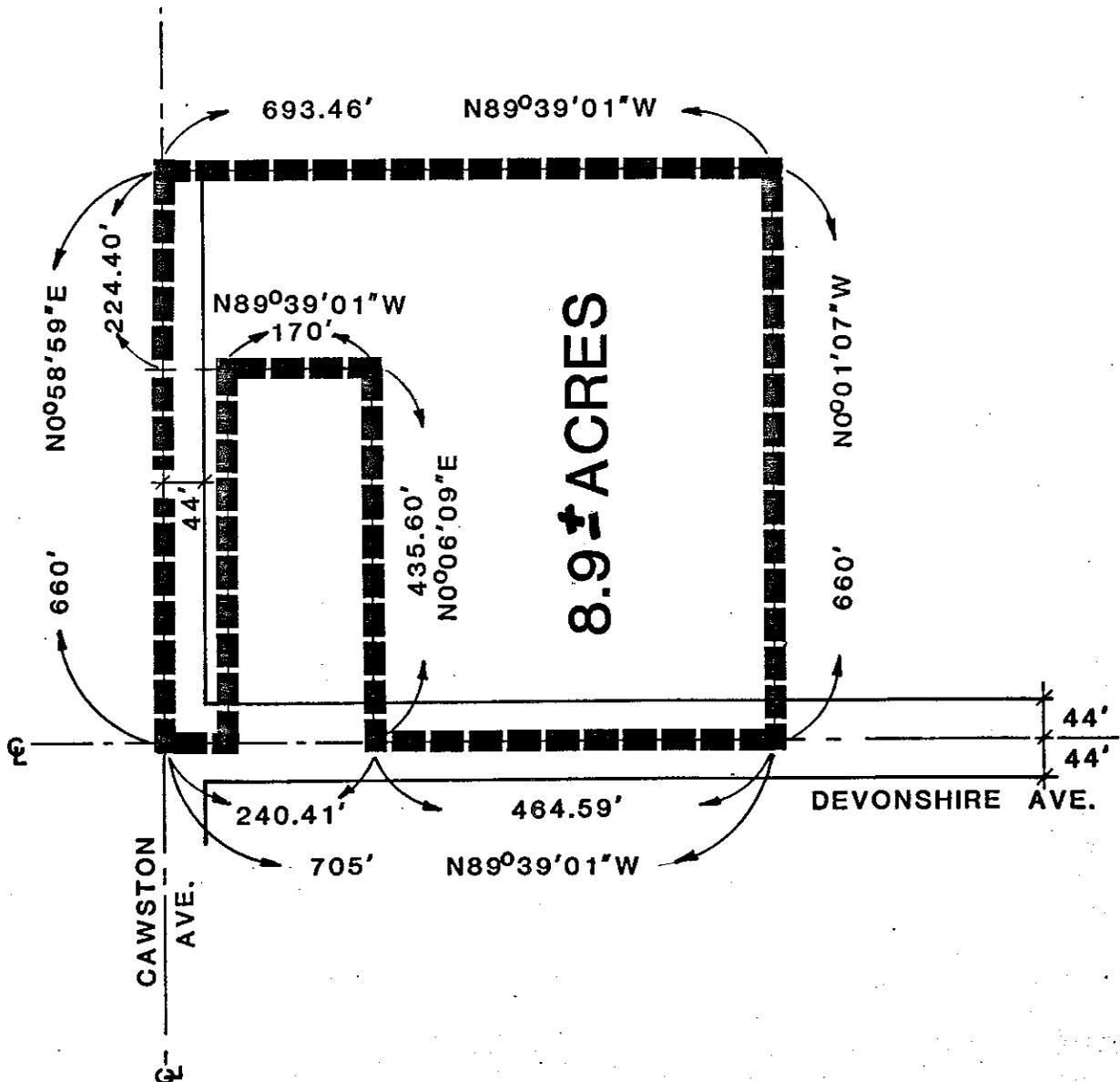
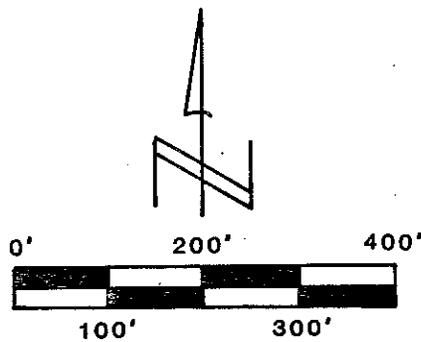
ZONE CHANGE NO.88-07

FROM: EXISTING DEVELOPMENT STANDARDS

TO: MODIFIED DEVELOPMENT STANDARDS

ORD. 1280

SUBJECT PROPERTY 



AS#112

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF HEMET, CALIFORNIA, APPROVING ZONE CHANGE NO. 87-14 WHEREIN THE PROPERTY GENERALLY LOCATED SOUTH OF MENLO AVENUE, BETWEEN CAWSTON AVENUE AND SANDERSON AVENUE IS HEREBY CHANGED FROM SPECIFIC PLAN 85-01 (TR-20 STANDARDS) TO SPECIFIC PLAN 85-01 SINGLE FAMILY (R-1-5, R-1-6 AND R-1-8 DEVELOPMENT STANDARDS)

SECTION ONE:

The official Zoning Map, Atlas Sheet Number 112 and 113, is hereby amended by changing the zone for the hereinafter hereinafter described property from Specific Plan 85-01 (TR-20 Standards) to Specific Plan 85-01 Single Family (R-1-5, R-1-6, and R-1-8 Standards)

112 ± acres located south of Menlo Avenue, between Cawston Avenue and Sanderson Avenue

(Legal description on file in the Department of Community Development).

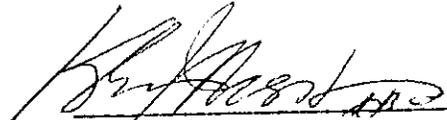
SECTION TWO:

A map of the property described in Section One herein is attached hereto as Exhibit "A".

Introduced at the regular meeting of August 25, 1987.

Enacted at the regular meeting of September 8, 1987 by the following vote:

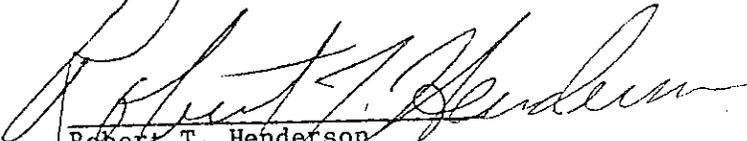
AYES: Council Members Baskett, Garrett, Herron, Nishino.
NOES: None.
ABSTAIN: None.
ABSENT: None.


Ken Nishino
Mayor, City of Hemet

ATTEST:


Edward J. Rodeghier
City Clerk, City of Hemet

APPROVED AS TO FORM:


Robert T. Henderson
City Attorney

Street Information and (2)
 (where listed)
 Scale: 1" = 100'
 Last updated:
 11/21/01 - Ord. # 1431

CITY OF HEMET ATLAS MAP

OFFICIAL ZONING MAP

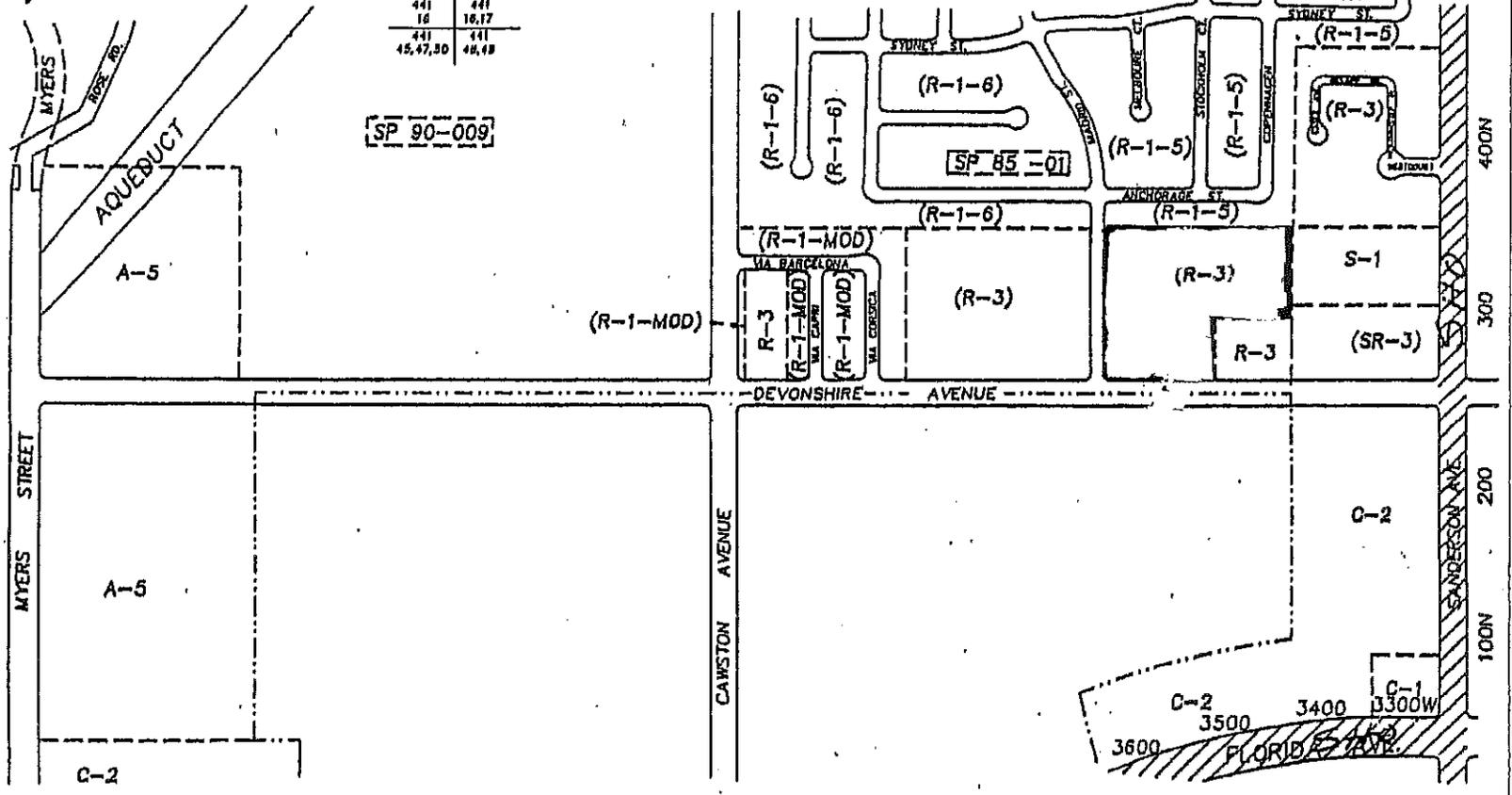
ADDRESS:
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AP BOOK/PAGE

441	441
16	16,17
441	441
45,47,50	46,48

SP 90-009



RESOLUTION NO. 87-16

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF HEMET,
CALIFORNIA, APPROVING TENTATIVE TRACT MAP NO. 22189

WHEREAS, an application for a Tentative Map was duly filed by:

APPLICANT: Spirit Development Company
OWNER: Arthofer Development

WHEREAS, the Applicant is requesting the Tentative Map to:

A tentative tract map to subdivide 104 ± acres into 472 lots.
Applicable Code Sections: Section 66410 et. seq. of the
California Government Code, (Subdivision Map Act); Section

of Title 17, the Hemet Zoning Ordinance; and Section 18000,
(Subdivisions), Title 16, of the Hemet Municipal Code.

WHEREAS, the property to be subdivided is described as follows:

104 ± acres located at the southwest corner of Menlo and
Sanderson

(Legal description on file in the Department of Community
Development)

WHEREAS, the Tentative Map was submitted to appropriate agencies
as required by Section 17.28.080 of the Municipal Code with a request for
their report and recommendations;

WHEREAS, the Planning Commission has received the report and
recommendation of such agencies as have submitted same including the
written report and recommendations of Staff;

WHEREAS, the Tentative Map was considered by the Commission on
the 21st day of January, 1987, at the hour of 7:00 p.m.; and

WHEREAS, all requirements of the California Environmental Quality
Act and the City's E.I.R. Guidelines have been met for the consideration of
whether the project will have a significant effect on the environment;

NOW, THEREFORE, for the reasons provided in the report and
recommendation of Staff, and subject to the Applicant's compliance with the
conditions attached hereto, the Planning Commission now finds as follows:

1. The proposed parcel and tract maps are consistent with the RII (7
du/ac) Land Use Designation of the General Plan.
2. The site, due to its flatness and location in the vicinity of major
shopping facilities, is physically suitable for single family homes.
It is close to shopping centers.
3. The design will not cause substantial environmental damage. Its
design will improve the drainage problems in the area. It has all the
other urban utilities necessary for residential needs.
4. There are no known easements which should conflict with the
development of this project.

PURSUANT TO THE ABOVE FINDING, IT IS RESOLVED that Tentative
Tract Map No. 22189 be approved subject to compliance with the Condition of
Approval in Exhibit "A" attached hereto and incorporated herein.

PASSED, APPROVED AND ADOPTED this 21st day of January, 1987,
the following vote:

AYES: Barton, Becker, Cain, Kivett, Kokes, Quinn, Thomas
NOES: None
ABSTAIN: None
ABSENT: None

Thornton Kivett, Chairman

ATTEST:

Mark Goldberg, Director of Community Development

PLANNING

1. The tentative tract map shall comply with the State of California Subdivision Map Act and the requirements of the City of Hemet City Code, Chapter 19, unless modified by the conditions listed below. This approved or conditionally approved tentative map will expire two years after the Planning Commission approval date of January 21, 1987, unless extended as provided by the Subdivision Map Act.
2. Lots created by this tentative map shall be in conformance with the Development Standards of the R-1 Zone.
3. Prior to the recordation of the Final Map, Zone Change No. 86-29 shall be approved by the City Council and shall be effective. Lots created by this land division shall be in conformance with the development standards of the zone ultimately applied to the property.
4. Phasing of this land division shall be subject to Planning Department approval.
5. The developer shall submit a School Mitigation Agreement letter to the Planning Department from the effected School District, stating that impacts upon the schools have been mitigated, per Title 18 (Development School Fees) of the Hemet Municipal Code.
6. The developer shall pay all fees for subdivision map checking.
7. Avigation Easements, in a form and content approved by the Planning Director and City Attorney, covering all lots, shall be recorded and granted to the City of Hemet prior to recording the Final Map. The granting of Avigation Easements shall be noted on the Final Tract Map. Said Easements shall be from runway ground elevation within 5,000 feet of the runway within the Approach Zones and all other areas shall be 150 feet above the runway ground level.
8. Prior to the issuance of building permits, common open space area improvement plans shall be submitted to the Planning Department for review and approval.
9. Access rights shall be waived along the south side of Street "A"; along both sides of Street "B" between "A" and "N"; along the south side of Street "H" between "B" and "Q"; along the east side of Cawston Avenue; along the south side of Menlo; and, along the west side of Sanderson Avenue.
10. Lot "A" Flood Control shall be fenced with six-foot high chain link fence.
11. Lot "A" Flood Control shall be maintained by an assessment district to be prepared in conformance with the City Attorney.

FIRE AND LIFE SAFETY SERVICES DIVISION

No comment

PUBLIC WORKS REQUIREMENTS

Preliminary City Ordinance requirements pertaining to the installation of offsite improvements and public parking lots.

Provide two sets of plans prepared by a registered Civil Engineer indicating the following improvements:

12. Sanderson Avenue

- A. Install type "B" curb and gutter on an alignment 38' west of center line per Standard No. C-201 and St-103.

- 1 B. Install street paving. Remove and replace existing pavement to
2 center line. Minimum pavement cross section to be 3" A.C. over 6"
3 A.B. Cl. III.
- 4 C. Install street lights per City standards and specifications.
- 5 D. Install water mains and water service(s) and sewer mains and sewer
6 laterals.
- 7 E. Install pavement taper on south side of project with W21-R guide
8 markers behind the taper.
- 9 F. Install 6.0' wide monolithic type sidewalk.
- 10 G. Install street trees 40' on center with automatic irrigation
11 system.
- 12 13. Intersection of Sanderson and Menlo
- 13 A. Install 35' minimum curb return, spandrel and cross gutter.
- 14 B. Install stop signs and street name signs.
- 15 C. Install handicapped ramp.
- 16 14. Menlo Avenue
- 17 A. Install type "B" curb and gutter on an alignment 32' south of
18 center line per Standard No. C-201 and St-103.
- 19 B. Install street paving. Remove and replace existing pavement to
20 center line. Minimum pavement cross section to be 3" A.C. over 6"
21 A.B. Cl. III.
- 22 C. Install street lights per City standards and specifications.
- 23 D. Install water mains and water service(s) and sewer mains and sewer
24 laterals.
- 25 E. Install 6.0' wide monolithic type sidewalk.
- 26 F. Install street trees 40' on center with automatic irrigation
27 system.
- 28 15. Intersection of Menlo & Cawston
- 29 A. Install 35' minimum curb return, spandrel and cross gutter.
- 30 B. Install stop signs and street name signs.
- 31 C. Install handicapped ramp.
- 32 16. Cawston Avenue
- 33 A. Install type "B" curb and gutter on an alignment 32' west of center
34 line per Standard No. C-201 and St-103.
- 35 B. Install street paving. Remove and replace existing pavement to
36 center line. Minimum pavement cross section to be 3" A.C. over 6"
37 A.B. Cl. III.
- 38 C. Install street lights per City standards and specifications.
- 39 D. Install water mains and water service(s) and sewer mains and sewer
40 laterals.
- 41 E. Install pavement taper on south side of project with W21-R guide
42 markers behind the taper.
- 43 F. Install 6.0' wide monolithic type sidewalk.
- 44 G. Install street trees 40' on center with automatic irrigation
45 system.
- 46 17. "A" & "B" Streets
- 47 A. Install curb, gutter, sidewalk and paving per City Standard No.
48 ST-103.
- 49 B. Minimum pavement cross section to be 3" A.C. over 6" A.B., Cl. III.
- 50 C. Install street lights per City standards and specifications.
- 51 D. Install water mains and water service(s) and sewer mains and sewer
52 laterals.
- 53 E. Install 6.0' wide monolithic type sidewalk.
- 54 F. Install street trees 40' on center with automatic irrigation
55 system.
- 56 G. Curb returns shall be a minimum of 25'. Cross gutters shall be a
57 minimum of 10' per Standard No. C-212 and C-213.
- 58 H. Install stop signs and street name signs.
- 59 I. Install handicapped ramp.

18. "C", "CC" & "DD" Streets

- A. Install curb, gutter, sidewalk and paving per City Standard No. ST-104.
- B. Minimum pavement cross section to be 3" A.C. over 6" A.B., Cl. III.
- C. Install street lights per City standards and specifications.
- D. Install water mains and water service(s) and sewer mains and sewer laterals.
- E. Install 6.0' wide monolithic type sidewalk.
- F. Install street trees 40' on center with automatic irrigation system.
- G. Curb returns shall be a minimum of 25'. Cross gutters shall be a minimum of 10' per Standard No. C-212 and C-213.
- H. Install stop signs and street name signs.
- I. Install handicapped ramp.

19. Local Streets

- A. Install curb, gutter, sidewalk and paving per City Standard No. ST-106.
- B. Minimum pavement cross section to be 3" A.C. over 6" A.B., Cl. III.
- C. Install street lights per City standards and specifications.
- D. Install water mains and water service(s) and sewer mains and sewer laterals.
- E. Install 6.0' wide monolithic type sidewalk.
- F. Install street trees 40' on center with automatic irrigation system.
- G. Curb returns shall be a minimum of 25'. Cross gutters shall be a minimum of 10' per Standard No. C-212 and C-213.
- H. Install stop signs and street name signs.
- I. Install handicapped ramp.
- J. All cul-de-sacs shall be installed per Standard No. G-800. Knuckle streets shall be drawn per Riverside County standards.

20. Drainage

- A. The incremental increase in runoff between the developed and undeveloped property for the 100-year/6-hour storm must be retained on site.
- B. The developer must contribute to the Master Storm Drain Plan in an amount recommended by the Public Works Department and set by the City Council.
- C. This property lies with H.U.D. Flood Insurance Rate Map Zone C; no extraordinary measures are needed to prevent property damage.
- D. Install West Hemet Master Flood Control Line 2-C.
- E. Lot "A" will not be accepted by the City of Hemet as a storm drain facility until arrangements have been made for on-going maintenance and protection from liability. This facility will eventually be maintained by Riverside County Flood Control District; therefore, District approval will be required.

21. Mapping

- A. A reproducible street map of the proposed subdivision on a scale of 1" = 400' and 1" = 800 feet shall be filed with the Public Works Department.
- B. Monumentation shall be in accordance with Standard Drawing M-900 and M-900A. In lieu of Standard Drawing M-901, street centerline monuments shall conform with Riverside County Surveyor's Office provided that cross-ties are set in top of curbs and tie sheets are filed with the City Engineer.

22. General

- A. Stop signs and street name signs shall be installed at all public street intersections. Sizes and locations will be determined by the Public Works Dept. when plans are submitted for check purposes.
- B. Lighting and Landscaping Maintenance Dist. requirements shall apply to the development.

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- C. Additional right of way will or may be required for street improvements.
- D. Drainage easements will be required.
- E. Install fire hydrant(s), if required by the Hemet Fire Dept. per Standards W-700 and W-700A.
- F. Provide detailed site grading and drainage plans supported by hydrology calculations for submittal to the City Flood Control Engineer.

23. Traffic Signal

- A. The developer shall install a traffic signal at the intersection of Devonshire and Sanderson, pursuant to the Traffic Signal Mitigation Fee Ordinance, for reimbursement procedures.

-END-
CITY OF HEMET
JANUARY 21, 1987